

ROAD TRAFFIC REGULATION ACT 1984

THE LONDON BOROUGH OF RICHMOND UPON THAMES (PROHIBITION OF
STOPPING ON CAB RANKS) (AMENDMENT No. 4) ORDER 2016
THE LONDON BOROUGH OF RICHMOND UPON THAMES
(SHARED USE) (LOADING PLACES AND TAXI RANKS) (No. 1) ORDER 2016
THE LONDON BOROUGH OF RICHMOND UPON THAMES (WAITING AND LOADING
RESTRICTION) (CIVIL ENFORCEMENT AREA) (AMENDMENT NO. 135) ORDER 2016

KEW ROAD, RICHMOND
THE QUADRANT, RICHMOND
(Reference 14/160)

1. NOTICE IS HEREBY GIVEN that the Council of the London Borough of Richmond upon Thames made the above mentioned Orders on 30 June 2016 under sections 6 and 124 of, and Part IV of Schedule 9 to the Road Traffic Regulation Act 1984, as amended.
2. The general effect of the **Cab Ranks Order** is to introduce a rank on the south-east side of Kew Road, outside Richmond station having a length of 14.2 metres and on the north-west side of Kew Road opposite Richmond station having a length of 85.8 metres. The sole purpose of this Order is to replicate and continue in force indefinitely the provisions of the London Borough Of Richmond upon Thames (Prohibition of Stopping on Cab Ranks) (No. 1) Experimental Order 2014
3. The general effect of the **Shared Use Order** is to introduce 2 areas of bays which will be used as loading bays during the day and as taxi ranks overnight. One area is on the north-west side of The Quadrant opposite the north-eastern kerb-line of Drummonds Place (with a length of 24 metres) and the other is on the opposite side (the south-east side of The Quadrant), fully on the footway and located to the south-west of its junction with Drummonds Place (with a length of 31.5 metres). The loading bays will be in operation between 7am and 7pm throughout the week, and the taxi ranks will be in operation between 7pm and 7am throughout the week. The sole purpose of this Order is to replicate and continue in force indefinitely the provisions of the London Borough of Richmond upon Thames (Shared Use) (Loading Places and Taxi Ranks) (No. 1) Experimental Order 2014
4. The general effect of the **Waiting and Loading Restriction Order** is to convert a 31.5 metre length of double yellow line to a single yellow line ("no waiting between 7pm and 7am throughout the week") to facilitate a "drop off and set down" area between 7am and 7pm throughout the week on the south-east side of The Quadrant, south-west of its junction with Drummonds Place. The sole purpose of this Order is to replicate and continue in force indefinitely the provisions of the London Borough of Richmond upon Thames (Waiting and Loading Restriction) (Civil Enforcement Area) (No. 7) Experimental Order 2014
5. Full details of the Orders, which will come into operation on 5 July 2016, are contained in the following documents:-
 - (a) the Orders;
 - (b) a plan indicating the location and effect of the restrictions; and
 - (c) the Council's Statement of Reasons for making the Orders.
6. Copies of the above documents:-

- (a) can be inspected, quoting Reference 14/160, at the Civic Centre (Central Reception, ground floor), 44 York Street, Twickenham between 9.15am and 5pm on Mondays to Fridays, except for Bank and other public holidays; and
 - (b) may be viewed on the Council's web-site at http://www.richmond.gov.uk/public_notices
7. If any person wishes to question the validity of these Orders or of any of their provisions on the grounds that they are not within the powers conferred by the Road Traffic Regulation Act 1984, or that any requirement of the said Act of 1984 or of any instrument made under the said Act has not been complied with, that person may, within 6 weeks from the date on which the Orders were made, apply for the purpose to the High Court.

GRAHAM BEATTIE
Assistant Director, Traffic and Transport
Civic Centre, 44 York Street, Twickenham TW1 3BZ

Dated 1 July 2016



TRAFFIC MANAGEMENT ORDER

THE LONDON BOROUGH OF RICHMOND UPON THAMES (WAITING AND LOADING RESTRICTION) (CIVIL ENFORCEMENT AREA) (AMENDMENT No. 135) ORDER 2016

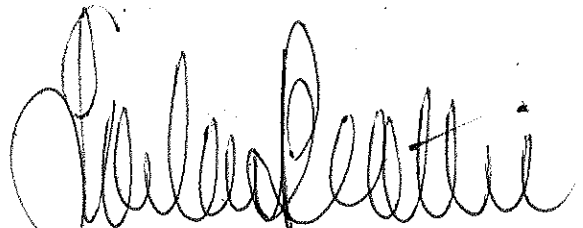
Made: 30 June 2016

Coming into operation: 5 July 2016

The Council of the London Borough of Richmond upon Thames, having consulted the Commissioner of Police of the Metropolis, in exercise of the powers conferred by Sections 6 and 124 of, and Part IV of Schedule 9 to, the Road Traffic Regulation Act 1984(a), as amended, hereinafter referred to as "the Act of 1984" and all other enabling powers hereby make the following Order —

1. This Order shall come into operation on 5 July 2016 and may be cited as the London Borough of Richmond upon Thames (Waiting and Loading Restriction) (Civil Enforcement Area) (Amendment No. 135) Order 2016.
2.
 - (1) In this Order the expression "enactment" means any enactment whether public, general or local and includes any order, bye-law, rule, regulation, scheme or other instrument having effect by virtue of an enactment.
 - (2) Any reference in this Order to any enactment shall be construed as a reference to that enactment as amended, applied, consolidated, re-enacted by or as having effect by virtue of any subsequent enactment.
 - (3) The Interpretation Act 1978(b) shall apply for the interpretation of this Order as it applies for the interpretation of an Act of Parliament.
3. Whilst this Order continues in force the provisions of the London Borough of Richmond upon Thames (Waiting and Loading Restriction) (Civil Enforcement Area) (Consolidation No. 8) Order 2008(c), as amended, shall have effect as though
 - (a) for the item numbered 551 In Schedule 1 to that Order there was substituted the item similarly numbered and set out in columns 1, 2 and 3 of Schedule 1 to this Order;
 - (b) there was added to Schedule 3 to that Order the item set out in columns 1, 2 and 3 of Schedule 2 to this Order.

Dated 30 June 2016



Assistant Director, Traffic and Transport
(The officer appointed for this purpose)

(a) 1984 c. 27. (b) 1978 c. 30.
(c) 2008/38 – There are no relevant amending Ordes

SCHEDULE 1
(See Article 3(a))

ITEM TO BE SUBSTITUTED TO SCHEDULE 1 [to 2008/38]

1 Item No.	2 Street	3 Prescribed Hours – See Schedule 3 (to 2008/38)
551	Quadrant The, Richmond:	
	(a) the south-east side:	
	(i) from its junction with Sheen Road to a point 39 metres south-west of the south-western kerb-line of Drummonds Place;	A
	(ii) from a point 39 metres south-west of the south-western kerb-line of Drummonds Place north-eastwards for a distance of 31.5 metres;	Z56
	(iii) from a point 7.5 metres south-west of the south-western kerb-line of Drummonds Place to its junction with Kew Road;	A
	(b) the north-west side.	A

SCHEDULE 2
(See Article 3(b))

**ITEM TO BE ADDED TO SCHEDULE 3 [to 2008/38]
PRESCRIBED AND RESTRICTED HOURS**

1	2	3
Prescribed Hours – see Schedule 1	Street	Restricted hours - see Schedule 2 [to 2008/38]
Z56	Between 7pm and 7am throughout the week.	82

EXPLANATORY NOTE

This Order amends a length of waiting restriction in The Quadrant, Richmond to prohibit waiting between 7pm and 7am throughout the week except for taxis.

TRAFFIC MANAGEMENT ORDER

THE LONDON BOROUGH OF RICHMOND UPON THAMES (SHARED USE) (LOADING PLACES AND TAXI RANKS) (No. 1) ORDER 2016

Made: 30 June 2016

Coming into operation: 5 July 2016

The Council of the London Borough of Richmond upon Thames, having consulted the Commissioner of Police of the Metropolis, in exercise of the powers conferred by Sections 6 and 124 of, and Part IV of Schedule 9 to, the Road Traffic Regulation Act 1984(a), as amended, and all other enabling powers hereby make the following Order —

1. This Order shall come into operation on 5 July 2016 and may be cited as the London Borough of Richmond upon Thames (Shared Use) (Loading Places and Taxi Ranks) (No. 1) Order 2016.

2. (1) In this order the following expressions have the meanings hereby respectively assigned to them:

“enactment” means any enactment whether public, general or local and includes any order, bye-law, rule, regulation, scheme or other instrument having effect by virtue of an enactment;

“the designated areas” means

- (a) that area of The Quadrant, Richmond on the north-west side of that street as lies between a point 5 metres north-east of the north-eastern kerb-line of Quadrant Road and a point 24 metres to the north-east thereof, bounded on one side by the edge of the carriageway and having a width throughout of 2.7 metres; and
- (b) that area of The Quadrant, Richmond on the south-east side of that street as lies between a point 7.5 metres south-west of the south-western kerb-line of and a point 31.5 metres to the south-west thereof, bounded on one side by the edge of the carriageway, being fully on the footway and having a width throughout of 2.0 metres;

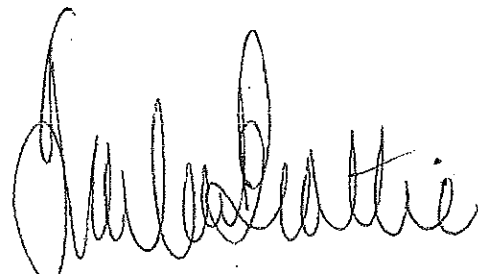
“the loading places order” means the London Borough of Richmond upon Thames (Loading Places) Order 1994(b) as amended;

“the cab ranks order” means the London Borough of Richmond upon Thames (Prohibition of Stopping on Cab Ranks) Order 1994(c) as amended;

(2) Any reference in this Order to any enactment shall be construed as a reference to that enactment as amended, applied, consolidated, re-enacted by or as having effect by virtue of any subsequent enactment.

- (3) The Interpretation Act 1978(d) shall apply for the interpretation of this Order as it applies for the interpretation of an Act of Parliament.
3. Whilst this Order continues in force the provisions of the loading places order shall apply to the designated parking place as if:
- (a) in Article 3(1) of that Order the expression "loading place" is deemed to include the designated areas;
 - (b) the "permitted hours" (as used in that order) relevant to the designated areas means the period between 7am and 7pm throughout the week;
 - (c) the designated areas were deemed to be included in the provisions of Article 5 of that order; and
 - (d) Articles 6 and 7 of that order were interpreted as if the maximum period for which a vehicle may be left in the designated areas is twenty minutes.
4. Whilst this Order continues in force the provisions of the cab ranks order shall apply to the designated parking place as if:
- (a) in Article 2(1) of that order the expression "cab rank" was deemed to include designated areas and that the relevant road marking was a traffic sign as shown Diagram 1028.4 of the Traffic Signs Regulations and General Directions 2016(e) but without any associated legend;
 - (b) the "prescribed hours" (as used in that order) relevant to the designated areas means the period between 7pm and 7am throughout the week; and
 - (c) the designated areas were included in the provisions of Article 3 of that order.

Dated: 30 June 2016



Assistant Director, Traffic and Transport
(The officer appointed for this purpose)

(a) 1984 c. 27.

(b) 1994/03.

(c) 1994/60.

(d) 1978 c. 30.

(e) S.I. 2016 No 362

EXPLANATORY NOTE

(This Note is not part of the Order but is intended to indicate its general purport.)

This Order introduces shared use parking places in The Quadrant, Richmond. During the day (between 7am and 7pm) the parking places will be loading bays subject to a maximum stay of 20 minutes, and overnight (between 7pm and 7am) they will be taxi ranks.

The Order was previously introduced on an experimental basis and the purpose of this Order is to continue in force the provisions of the Order indefinitely.



TRAFFIC MANAGEMENT ORDER

THE LONDON BOROUGH OF RICHMOND UPON THAMES (PROHIBITION OF STOPPING ON CAB RANKS) (AMENDMENT No. 4) ORDER 2016

Made: 30 June 2016

Coming into operation: 5 July 2016

The Council of the London Borough of Richmond upon Thames, having consulted the Commissioner of Police of the Metropolis, in exercise of the powers conferred by sections 6 and 124 of the Road Traffic Regulation Act 1984(a), as amended, (hereinafter referred to as "the Act of 1984") and all other enabling powers hereby makes the following order:

Citation and commencement

1. This Order shall come into operation on 5 July 2016 and may be cited as the London Borough of Richmond upon Thames (Prohibition of Stopping on Cab Ranks) (Amendment No. 4) Order 2016.

Interpretation

2. (1) In this order the following expressions have the meanings hereby respectively assigned to them:

"cab rank" means the area within or adjacent to the length of the road specified in column 2 of the table in Schedule 1 to this Order being an area appointed as a standing for hackney carriages under section 4 of the London Hackney Carriages Act 1850(b) and bounded by a traffic sign as shown in diagram 1028.2 of Schedule 6 to the Traffic Signs Regulations and General Directions 2002(c), as amended, and varied by the addition of a continuous yellow line 200 or 300 millimetres wide along the edge of the said length of carriageway;

"civil enforcement officer" means a person appointed by or on behalf of the Council for the enforcement of road traffic contraventions;

"Council" means the Council of the London Borough of Richmond upon Thames;

"electronic communication apparatus" has the same meaning as in the Communications Act 2003(d);

"licensed cab" means a hackney carriage in respect of which there is in force a licence granted under section 6 of the Metropolitan Public Carriage Act 1869(e) and the expression "cab" shall take the same meaning;

"penalty charge" means the charge set by the Council under the provisions of the Traffic Management Act 2004(f) and following the approval of the Mayor of London, which is to be paid to the Council following the issue of a penalty charge notice and within 28 days of the issue of that notice;

(a) 1984 c. 27.
(d) 2003 No. 21.

(b) 1850 c. vii.
(e) 1869 c. 115.

(c) S.I. 2002 No. 3113
(f) 2004 c. 18.

“prescribed hours” in relation to a cab rank means the times specified in column 4 of the schedule to this order;

“reduced penalty charge” means the charge set by the Council under the provisions of the Traffic Management Act 2004 and following the approval of the Mayor of London, which is to be paid to the Council following the issue of a penalty charge notice within 14 or 21 days of the issue of that notice;

- (2) For the purposes of this Order, a vehicle shall be deemed to be stopped on a cab rank:
- (a) if any point in that area is below the vehicle or its load (if any) and the vehicle is stationary, or
 - (b) for more than a specified period if any point in that area is below the vehicle or its load (if any) throughout a period exceeding the specified period whether or not the vehicle is moved during that period.

Prohibition of stopping on a cab rank

3. No person shall cause or permit any vehicle to stop during the prescribed hours on the lengths of roads designated as cab ranks and set out in column 2 of Schedule 1 to this Order.

Contraventions in a cab rank

4. Except as provided in Article 6 or Article 7 of this order, if a vehicle stops during the prescribed hours on a cab rank set out in column 2 of the table in the Schedule to this order, a contravention shall have occurred and a penalty charge shall be payable. A penalty charge notice showing the information required by the Traffic Management Act 2004 may then be issued by a civil enforcement officer.

Manner of payment of the penalty charge

5. The penalty charge shall be paid as provided for by Schedule 2 to the London Borough of Richmond upon Thames (Manner of Payment of Penalty Charges) Order 2011(a).

Exemptions

6. Nothing in Article 3 of this Order shall apply in relation to
- (a) a licensed cab; or
 - (b) a vehicle being used for fire brigade, ambulance or police purposes; or
 - (c) any vehicle being used in the service of a local authority in pursuance of statutory powers provided that in all the circumstances it is reasonably necessary in the exercise of such powers or the performance of such duties for the vehicle to be stopped on the cab rank; or
 - (d) any vehicle being used in connection with any building operation, demolition or excavation on or adjacent to the cab rank, the removal of any obstruction to traffic on or adjacent to the cab rank, the maintenance, improvement or reconstruction of the road in which the cab rank is situated, the laying, erection or repair on or adjacent to the cab rank of any sewer, or of any main, pipe or apparatus for the

(a) 2011/11.

supply of gas, water or electricity, or of any electronic communication apparatus or the placing, maintenance or removal of any traffic sign if the vehicle cannot conveniently and lawfully be used for that purpose in any part of any street which is not a cab rank or outside the prescribed hours.

7. Nothing in Article 3 of this Order shall apply

- (a) in any case where the person in control of the vehicle is required by law to stop on the cab rank, or is obliged to do so in order to avoid an accident, or
- (b) to anything done with the permission or at the direction of a police officer in uniform, a civil enforcement officer or a police community support officer, or
- (c) to any person who causes any vehicle to proceed in accordance with any restriction or requirement indicated by traffic signs placed pursuant to Section 66 or 67 of the Act of 1984

Manner of standing in a cab rank

8. The driver of a licensed cab waiting in a cab rank set out in column 2 of the table in Schedule 1 to this order during the prescribed hours and in accordance with the foregoing provisions of this Order shall cause it so to stand:

- (a) in accordance with the conditions (if any) specified in column 3 of any item in Schedule 1 to this order and
- (b) that the left or near side of the vehicle is adjacent to the left-hand edge of the carriageway;
- (c) that the distance between the edge of the carriageway and the furthest near-side wheel of the vehicle is not more than 300 millimetres and;
- (d) that every part of the vehicle is within the limits of the cab rank.

Alteration of position of a vehicle in a cab rank

9. A police officer in uniform, a traffic warden or a police community support officer or a civil enforcement officer may move or cause to be moved, in case of emergency, to any place they think fit, any vehicle which is left unattended in a cab rank.

10. Where any vehicle is standing in a cab rank in contravention of the provisions of Article 8 a civil enforcement officer may alter or cause to be altered the position of the vehicle in order that its position shall comply with those provisions.

Removal of a vehicle from a cab rank

11. Where a civil enforcement officer is of the opinion that any vehicle is stopped in a cab rank is other than a vehicle included the exemptions or exemptions included in Articles 6 and 7 of this Order he may remove or cause to be removed the vehicle from the cab rank and, where it is so removed, shall provide for the safe custody of the vehicle.

Placing of traffic signs

12. The Council shall place and maintain in or in the vicinity of each cab rank traffic signs and carriageway markings of the size, colour and type prescribed by or authorised by the Department for Transport indicating that such cab ranks may be used during the prescribed hours only for the waiting of licensed cabs.

Revocation of existing cab ranks

- 13. The cab ranks as specified in column 2 of Schedule 2 to this order and as provided for by the orders specified in column 4 are revoked.

Consequential amendments to other orders

- 14. The provisions of other orders (particulars of which are given in columns 2, 3 and 4 of any item in Schedule 3 to this order) shall either be amended or the whole of that item shall be revoked as specified in column 5 of that item.

Dated 30 June 2016



Assistant Director, Traffic and Transport
(The officer appointed for this purpose)

SCHEDULE 1
(See Articles 2 and 3)

CAB RANKS

1 Item No	2 Description of cab rank	3 Special manner of standing	4 Prescribed hours	5 Other special conditions
1	Kew Road, Richmond: the north-west side, from a point 0.2 metres north- east of the north-eastern flank wall of "Midmoor House", Kew Road north-eastwards for a distance of 85.8 metres.	Cabs to face north-east	At any time	None
2	Kew Road, Richmond: the south-east side, from the south-western limit of the layby outside No 27 The Quadrant north- eastwards for a distance of 14.2 metres.	Cabs to face south-west	At any time	None

SCHEDULE 2
(See Article 13)

SUSPENSION OF EXISTING CAB RANKS

1 Item No	2 Description of cab rank	3 Prescribed hours	4 Reference to other traffic management order
1	Kew Road, Richmond: the south-east side, from a point opposite the party wall of Nos 17 and 19 Kew Road north-eastwards for a distance of 46.5 metres.	At any time	1994/60, Schedule item 3(a)
2	Kew Road, Richmond: the north-west side, from a point opposite the party wall of Nos 1 and 3 Kew Road north-eastwards for a distance of 20 metres.	Between 10am and midnight and between midnight and 7am on Mondays to Fridays inclusive and at any time on Saturdays and Sundays	1994/60, Schedule item 3(b)

Note to Schedule 2

TMO 1994/60 is the London Borough of Richmond upon Thames (Prohibition of Stopping on Cab Ranks) Order 1994.

SCHEDULE 3
(See Article 14)

CONSEQUENTIAL AMENDMENTS TO OTHER TRAFFIC MANAGEMENT ORDERS

1 Item No	2 Reference to other traffic management order	3 Description of bus lane	4 Prescribed hours	5 Nature of amendment
1	TMO 2005/70, Schedule 1, item 8(a)	Kew Road, Richmond, the north-west side, South-west to north-east with-flow bus lane	Between 7am and 7pm throughout the week	The item specified in column 2 is revoked
2	TMO 2005/70, Schedule 1, item 8(b)(i)	Kew Road, Richmond, the south-east side, north-east to south-west with-flow bus lane	Between 7am and 7pm throughout the week	The item specified in column 2 is revoked.
3	TMO 2005/70, Schedule 1, item 14(a)	The Quadrant, Richmond, the north-west side, South-west to north-east with-flow bus lane	Between 7am and 7pm throughout the week	The item specified in column 2 is revoked
4	TMO 2005/70, Schedule 1, item 14(b)	The Quadrant, Richmond, the south-east side, north-east to south-west with-flow bus lane	Between 7am and 7pm throughout the week	The item specified in column 2 is revoked.

Note to Schedule 3

TMO 2005/70 is the London Borough of Richmond upon Thames (Reserved Bus Lanes) (Consolidation) Order 2005.

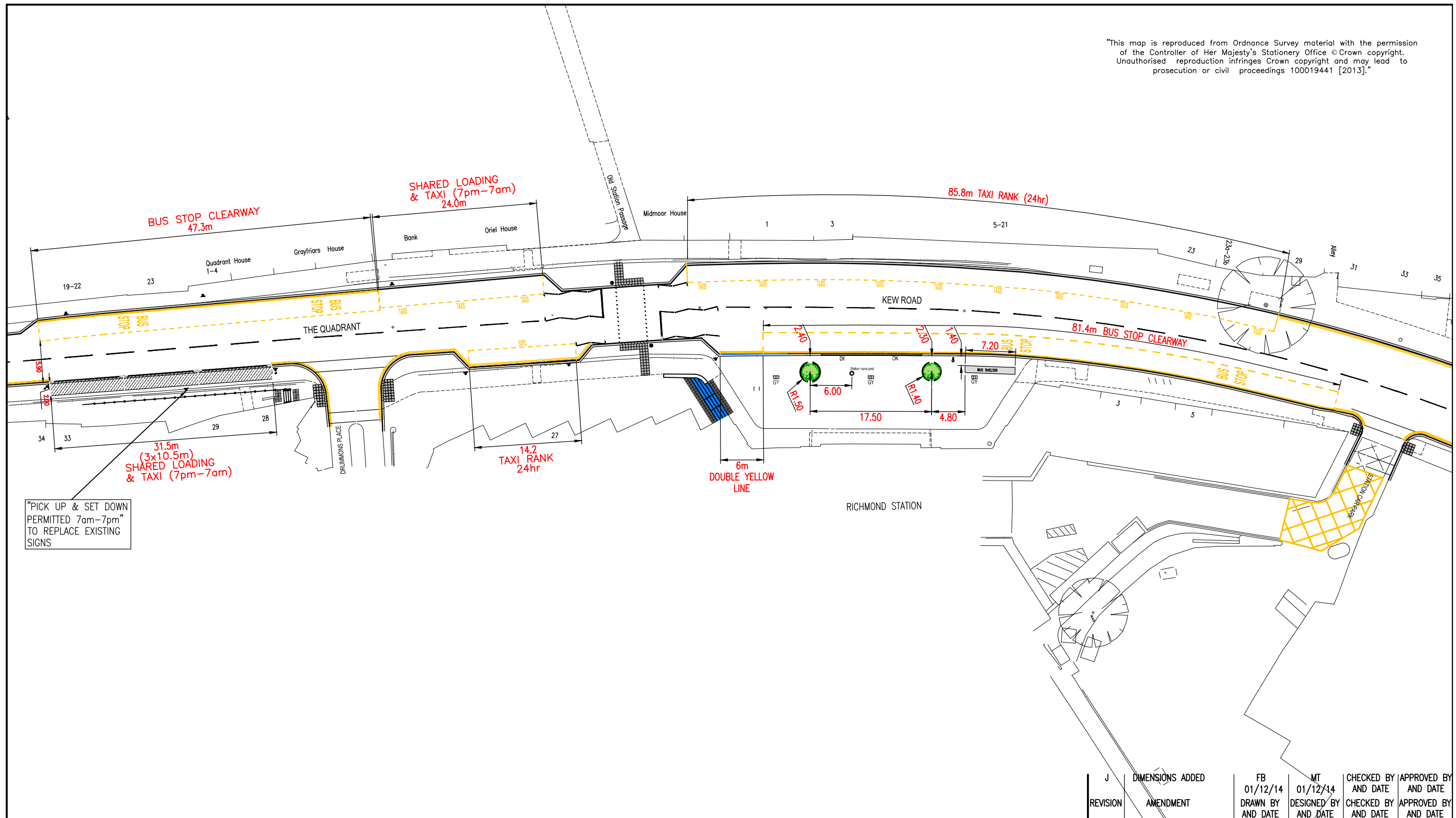
EXPLANATORY NOTE

(This note is not part of the Order but is intended to indicate its general purport)

This Order introduces two taxi ranks and discontinues two existing taxi ranks.

It is convenient that this order makes provision for the revocation of the two bus lanes on Kew Road and The Quadrant outside Richmond station. The Order was previously introduced on an experimental basis and the purpose of this Order is to continue in force the provisions of the Order indefinitely.

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"PICK UP & SET DOWN PERMITTED 7am-7pm" TO REPLACE EXISTING SIGNS

J	DIMENSIONS ADDED	FB	MT	CHECKED BY	APPROVED BY
REVISION	AMENDMENT	01/12/14	01/12/14	AND DATE	AND DATE
		DRAWN BY	DESIGNED BY	CHECKED BY	APPROVED BY
		AND DATE	AND DATE	AND DATE	AND DATE

		<input checked="" type="checkbox"/> DRAFT <input type="checkbox"/> ISSUE	SCALE (AT A3 SIZE) NTS
CLIENT	LONDON BOROUGH OF RICHMOND UPON THAMES		
PROJECT	RICHMOND TOWN CENTRE		
DRAWING TITLE	PROPOSED SIGNAGE/MARKINGS ALTERATIONS		
ISSUING OFFICE	TWICKENHAM	DRAWING NUMBER	3010.DD.1200.002
TELEPHONE	08456 122 660	REVISION	J

UNCONTROLLED

STATEMENT OF REASONS

RICHMOND STATION TAXI RANKS AND SHARED USE TAXI RANKS AND LOADING BAYS

The Council, in consultation with representatives of taxi operators in the borough, Transport for London, South West Trains and other interested parties is making changes to the road layout in the vicinity of Richmond Station. This will involve significant changes to bus stops, taxi ranks and loading bays. The orders introduce new cab ranks and shared use cab ranks and loading bays. These measures provide an improved and safer environment for all road users and passengers using the local public transport links.

ROAD TRAFFIC REGULATION ACT 1984

THE LONDON BOROUGH OF RICHMOND UPON THAMES (PROHIBITION OF
STOPPING ON CAB RANKS) (No. 1) EXPERIMENTAL ORDER 2014
THE LONDON BOROUGH OF RICHMOND UPON THAMES
(SHARED USE) (LOADING PLACES AND TAXI RANKS) (No. 1) EXPERIMENTAL
ORDER 2014
THE LONDON BOROUGH OF RICHMOND UPON THAMES (WAITING AND LOADING
RESTRICTION) (CIVIL ENFORCEMENT AREA) (No. 7) EXPERIMENTAL ORDER 2014

KEW ROAD, RICHMOND
THE QUADRANT, RICHMOND
(Reference 14/160)

1. NOTICE IS HEREBY GIVEN that the Council of the London Borough of Richmond upon Thames made the above mentioned Orders on 11 December 2014 under sections 9 and 10 of the Road Traffic Regulation Act 1984, as amended.
2. The general effect of the **Cab Ranks Order** is to introduce a rank on the south-east side of Kew Road, outside Richmond station having a length of 14.2 metres and on the north-west side of Kew Road opposite Richmond station having a length of 85.8 metres.
3. The general effect of the **Shared Use Order** is to introduce 2 areas of bays which will be used as loading bays during the day and as taxi ranks overnight. One area is on the north-west side of The Quadrant opposite the north-eastern kerb-line of Drummonds Place (with a length of 24 metres) and the other is on the opposite side (the south-east side of The Quadrant), fully on the footway and located to the south-west of its junction with Drummonds Place (with a length of 31.5 metres). The loading bays will be in operation between 7am and 7pm throughout the week, and the taxi ranks will be in operation between 7pm and 7am throughout the week.
4. The general effect of the **Waiting and Loading Restriction Order** is to convert a 31.5 metre length of double yellow line to a single yellow line ("no waiting between 7pm and 7am throughout the week") to facilitate a "drop off and set down" area between 7am and 7pm throughout the week on the south-east side of The Quadrant, south-west of its junction with Drummonds Place.
5. Full details of the Orders, which will come into operation on 5 January 2015, are contained in the following documents:-
 - (a) the Orders;
 - (b) a plan indicating the location and effect of the restrictions; and
 - (c) the Council's Statement of Reasons for making the Orders.
6. Copies of the above documents:-
 - (a) can be inspected, quoting Reference 14/160, at the Civic Centre (Central Reception, ground floor), 44 York Street, Twickenham between 9.15am and 5pm on Mondays to Fridays, except for Bank and other public holidays; and
 - (b) may be viewed on the Council's web-site at http://www.richmond.gov.uk/public_notices

until the date on which the Orders cease to have effect.

7. Copies of the Orders may be purchased from the Environment Directorate, Civic Centre, 44 York Street, Twickenham TW1 3BZ.
8. These Orders provide that in pursuance of section 10(2) of the Road Traffic Regulation Act 1984 the Assistant Director, Traffic and Transport of the London Borough of Richmond upon Thames, or other person authorised in that behalf by that officer may, if it appears to that officer or to that person essential in the interests of the expeditious, convenient and safe movement of traffic, or of the provision of suitable and adequate parking facilities on the highway, or for preserving or improving the amenities of the area through which the roads affected by these Orders run, modify or suspend the Orders or any provision thereof.
9. If the provisions of the Orders continue in operation for a period of not less than six months, the Council will consider in due course whether the provisions of the Orders should be reproduced and continued in force indefinitely by means of Orders under section 6 of the said Act of 1984. Persons wishing to object to or support the making of such Orders for the purpose of such reproduction and continuation in force may, within the period of six months referred to above (i.e. by 10 July 2015), send a statement in writing of their objection and the grounds of that objection to the address below.
10. Please address any representations to the Orders to the Highway Infrastructure Manager, London Borough of Richmond upon Thames, Civic Centre, 44 York Street, Twickenham, TW1 3BZ, or by email to highwaysandtransport@richmond.gov.uk quoting the deference given above by no later than 10 July 2015.
11. If any person wishes to question the validity of these Orders or of any of their provisions on the grounds that they are not within the powers conferred by the Road Traffic Regulation Act 1984, or that any requirement of the said Act of 1984 or of any instrument made under the said Act has not been complied with, that person may, within 6 weeks from the date on which the Orders were made, apply for the purpose to the High Court.
12. Other changes to the traffic management outside Richmond station not included in the Orders are the relocation of bus stops and an extension to the bus stop clearway.

ANDREW DARVILL
Assistant Director, Traffic and Transport
Civic Centre, 44 York Street, Twickenham TW1 3BZ

Dated 12 December 2014



TRAFFIC MANAGEMENT ORDER

THE LONDON BOROUGH OF RICHMOND UPON THAMES (PROHIBITION OF STOPPING ON CAB RANKS) (No. 1) EXPERIMENTAL ORDER 2014

Made: 11 December 2014

Coming into operation: 5 January 2015

The Council of the London Borough of Richmond upon Thames, having consulted the Commissioner of Police of the Metropolis, in exercise of the powers conferred by sections 9 and 10 of the Road Traffic Regulation Act 1984(a), as amended, (hereinafter referred to as "the Act of 1984") and all other enabling powers hereby makes the following order:

Citation and commencement

1. This Order shall come into operation on 5 January 2015 and may be cited as the London Borough of Richmond upon Thames (Prohibition of Stopping on Cab Ranks) (No. 1) Experimental Order 2014.

Interpretation

2. (1) In this order the following expressions have the meanings hereby respectively assigned to them:

"cab rank" means the area within or adjacent to the length of the road specified in column 2 of the table in Schedule 1 to this Order being an area appointed as a standing for hackney carriages under section 4 of the London Hackney Carriages Act 1850(b) and bounded by a traffic sign as shown in diagram 1028.2 of Schedule 6 to the Traffic Signs Regulations and General Directions 2002(c), as amended, and varied by the addition of a continuous yellow line 200 or 300 millimetres wide along the edge of the said length of carriageway;

"civil enforcement officer" means a person appointed by or on behalf of the Council for the enforcement of road traffic contraventions;

"Council" means the Council of the London Borough of Richmond upon Thames;

"electronic communication apparatus" has the same meaning as in the Communications Act 2003(d);

"licensed cab" means a hackney carriage in respect of which there is in force a licence granted under section 6 of the Metropolitan Public Carriage Act 1869(e) and the expression "cab" shall take the same meaning;

"penalty charge" means the charge set by the Council under the provisions of the Traffic Management Act 2004(f) and following the approval of the Mayor of London, which is to be paid to the Council following the issue of a penalty charge notice and within 28 days of the issue of that notice;

(a) 1984 c. 27.

(b) 1850 c. vii.

(c) S.I. 2002 No. 3113

(d) 2003 No. 21.

(e) 1869 c. 115.

(f) 2004 c. 18.

“prescribed hours” in relation to a cab rank means the times specified in column 4 of the schedule to this order;

“reduced penalty charge” means the charge set by the Council under the provisions of the Traffic Management Act 2004 and following the approval of the Mayor of London, which is to be paid to the Council following the issue of a penalty charge notice within 14 or 21 days of the issue of that notice;

- (2) For the purposes of this Order, a vehicle shall be deemed to be stopped on a cab rank:
 - (a) if any point in that area is below the vehicle or its load (if any) and the vehicle is stationary, or
 - (b) for more than a specified period if any point in that area is below the vehicle or its load (if any) throughout a period exceeding the specified period whether or not the vehicle is moved during that period.
- (3) In pursuance of section 10(2) of the Road Traffic Regulation Act 1984 the Assistant Director, Traffic and Transport of the London Borough of Richmond upon Thames, as the specified officer, or some person authorised in that behalf by that officer may, if it appears to that officer or to that person essential in the interests of the expeditious, convenient and safe movement of traffic, or of the provision of suitable and adequate parking facilities on the highway, or for preserving or improving the amenities of the area through which the road affected by this Order runs, modify or suspend this Order or any provision thereof.

Prohibition of stopping on a cab rank

3. No person shall cause or permit any vehicle to stop during the prescribed hours on the lengths of roads designated as cab ranks and set out in column 2 of Schedule 1 to this Order.

Contraventions in a cab rank

4. Except as provided in Article 6 or Article 7 of this order, if a vehicle stops during the prescribed hours on a cab rank set out in column 2 of the table in the Schedule to this order, a contravention shall have occurred and a penalty charge shall be payable. A penalty charge notice showing the information required by the Traffic Management Act 2004 may then be issued by a civil enforcement officer.

Manner of payment of the penalty charge

5. The penalty charge shall be paid as provided for by Schedule 2 to the London Borough of Richmond upon Thames (Manner of Payment of Penalty Charges) Order 2011(a).

Exemptions

6. Nothing in Article 3 of this Order shall apply in relation to
 - (a) a licensed cab; or
 - (b) a vehicle being used for fire brigade, ambulance or police purposes; or
 - (c) any vehicle being used in the service of a local authority in pursuance of statutory powers provided that in all the circumstances it is reasonably necessary in the

(a) 2011/11.

exercise of such powers or the performance of such duties for the vehicle to be stopped on the cab rank; or

- (d) any vehicle being used in connection with any building operation, demolition or excavation on or adjacent to the cab rank, the removal of any obstruction to traffic on or adjacent to the cab rank, the maintenance, improvement or reconstruction of the road in which the cab rank is situated, the laying, erection or repair on or adjacent to the cab rank of any sewer, or of any main, pipe or apparatus for the supply of gas, water or electricity, or of any electronic communication apparatus or the placing, maintenance or removal of any traffic sign if the vehicle cannot conveniently and lawfully be used for that purpose in any part of any street which is not a cab rank or outside the prescribed hours.

7. Nothing in Article 3 of this Order shall apply

- (a) in any case where the person in control of the vehicle is required by law to stop on the cab rank, or is obliged to do so in order to avoid an accident, or
- (b) to anything done with the permission or at the direction of a police officer in uniform, a civil enforcement officer or a police community support officer, or
- (c) to any person who causes any vehicle to proceed in accordance with any restriction or requirement indicated by traffic signs placed pursuant to Section 66 or 67 of the Act of 1984

Manner of standing in a cab rank

8. The driver of a licensed cab waiting in a cab rank set out in column 2 of the table in Schedule 1 to this order during the prescribed hours and in accordance with the foregoing provisions of this Order shall cause it so to stand:

- (a) in accordance with the conditions (if any) specified in column 3 of any item in Schedule 1 to this order and
- (b) that the left or near side of the vehicle is adjacent to the left-hand edge of the carriageway;
- (c) that the distance between the edge of the carriageway and the furthest near-side wheel of the vehicle is not more than 300 millimetres and;
- (d) that every part of the vehicle is within the limits of the cab rank.

Alteration of position of a vehicle in a cab rank

9. A police officer in uniform, a traffic warden or a police community support officer or a civil enforcement officer may move or cause to be moved, in case of emergency, to any place they think fit, any vehicle which is left unattended in a cab rank.

10. Where any vehicle is standing in a cab rank in contravention of the provisions of Article 8 a civil enforcement officer may alter or cause to be altered the position of the vehicle in order that its position shall comply with those provisions.

Removal of a vehicle from a cab rank

11. Where a civil enforcement officer is of the opinion that any vehicle is stopped in a cab rank is other than a vehicle included the exemptions or exemptions included in Articles 6 and 7

of this Order he may remove or cause to be removed the vehicle from the cab rank and, where it is so removed, shall provide for the safe custody of the vehicle.

Placing of traffic signs

12. The Council shall place and maintain in or in the vicinity of each cab rank traffic signs and carriageway markings of the size, colour and type prescribed by or authorised by the Department for Transport indicating that such cab ranks may be used during the prescribed hours only for the waiting of licensed cabs.

Suspension of existing cab ranks

13. Whilst this order remains in force, the cab ranks as specified in column 2 of Schedule 2 to this order and as provided for by the orders specified in column 4 shall be suspended.

Consequential amendments to other orders

14. Whilst this order remains in force, the provisions of other orders (brief particulars of which are given in columns 2, 3 and 4 of any item in Schedule 3 to this order) shall either be amended or the whole of that item shall be suspended as specified in column 5 of that item.

Dated 11 December 2014



Assistant Director, Traffic and Transport
(The officer appointed for this purpose)

SCHEDULE 1
(See Articles 2 and 3)

CAB RANKS

1 Item No	2 Description of cab rank	3 Special manner of standing	4 Prescribed hours	5 Other special conditions
1	Kew Road, Richmond: the north-west side, from a point 0.2 metres north- east of the north-eastern flank wall of "Midmoor House", Kew Road north-eastwards for a distance of 85.8 metres.	Cabs to face north-east	At any time	None
2	Kew Road, Richmond: the south-east side, from the south-western limit of the layby outside No 27 The Quadrant north- eastwards for a distance of 14.2 metres.	Cabs to face south-west	At any time	None

SCHEDULE 2
(See Article 13)

SUSPENSION OF EXISTING CAB RANKS

1 Item No	2 Description of cab rank	3 Prescribed hours	4 Reference to other traffic management order
1	Kew Road, Richmond: the south-east side, from a point opposite the party wall of Nos 17 and 19 Kew Road north-eastwards for a distance of 46.5 metres.	At any time	1994/60, Schedule item 3(a)
2	Kew Road, Richmond: the north-west side, from a point opposite the party wall of Nos 1 and 3 Kew Road north-eastwards for a distance of 20 metres.	Between 10am and midnight and between midnight and 7am on Mondays to Fridays inclusive and at any time on Saturdays and Sundays	1994/60, Schedule item 3(b)

Note to Schedule 2

TMO 1994/60 is the London Borough of Richmond upon Thames (Prohibition of Stopping on Cab Ranks) Order 1994.

SCHEDULE 3
(See Article 14)

CONSEQUENTIAL AMENDMENTS TO OTHER TRAFFIC MANAGEMENT ORDERS

1 Item No	2 Reference to other traffic management order	3 Description of bus lane	4 Prescribed hours	5 Nature of amendment
1	TMO 2005/70, Schedule 1, item 8(a)	Kew Road, Richmond, the north-west side, South-west to north-east with-flow bus lane	Between 7am and 7pm throughout the week	The item specified in column 2 to be suspended
2	TMO 2005/70, Schedule 1, item 8(b)(i)	Kew Road, Richmond, the south-east side, north-east to south-west with-flow bus lane	Between 7am and 7pm throughout the week	The item specified in column 2 to be suspended
3	TMO 2005/70, Schedule 1, item 14(a)	The Quadrant, Richmond, the north-west side, South-west to north-east with-flow bus lane	Between 7am and 7pm throughout the week	The item specified in column 2 to be suspended
4	TMO 2005/70, Schedule 1, item 14(b)	The Quadrant, Richmond, the south-east side, north-east to south-west with-flow bus lane	Between 7am and 7pm throughout the week	The item specified in column 2 to be suspended

Note to Schedule 3

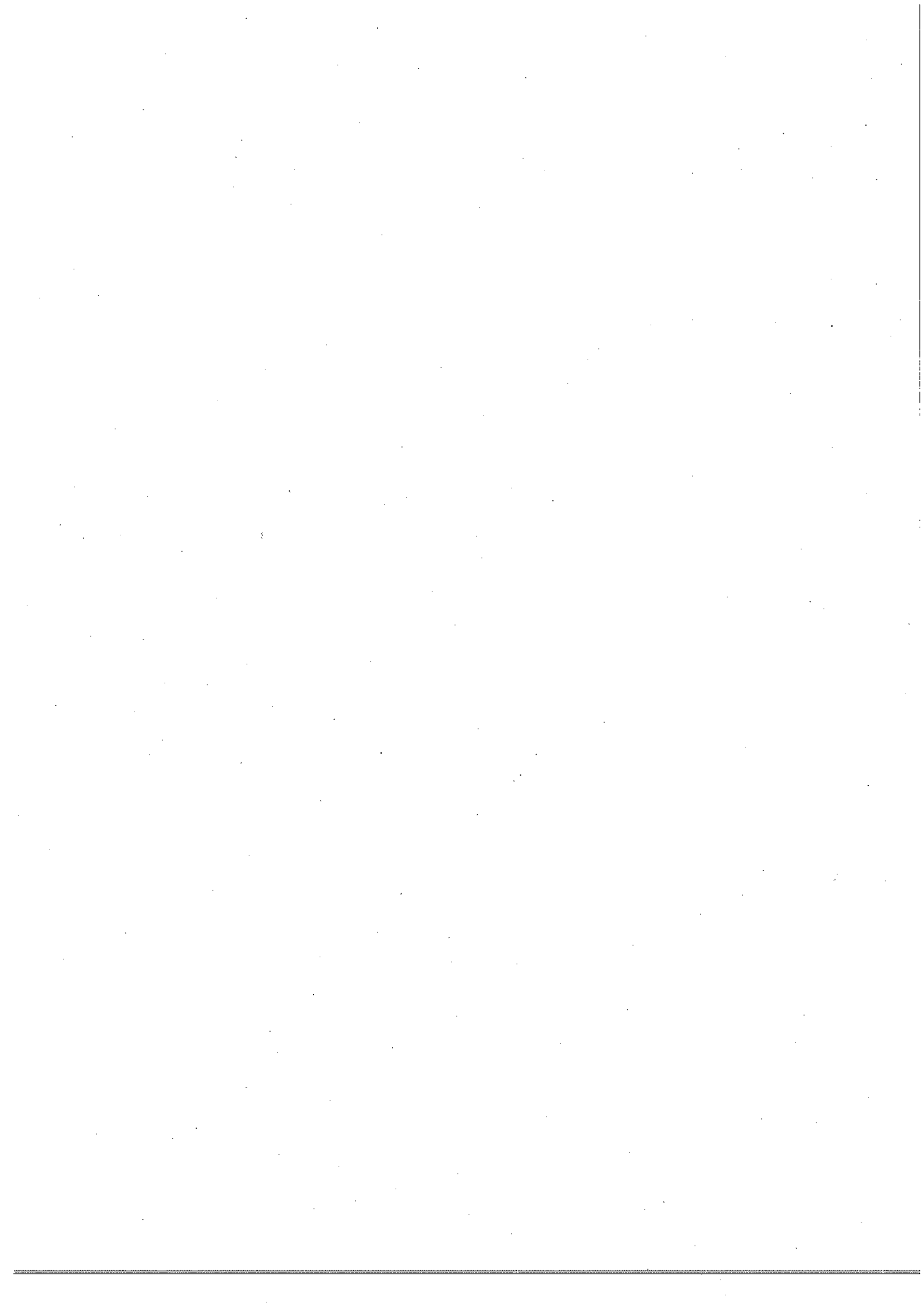
TMO 2005/70 is the London Borough of Richmond upon Thames (Reserved Bus Lanes) (Consolidation) Order 2005.

EXPLANATORY NOTE

(This note is not part of the Order but is intended to indicate its general purport)

This Order introduces two taxi ranks and discontinues two existing taxi ranks.

It is convenient that this order makes provision for the suspension of the two bus lanes on Kew Road and The Quadrant outside Richmond station. If the scheme is made permanent a separate order will be needed to revoke these bus lanes.



TRAFFIC MANAGEMENT ORDER

THE LONDON BOROUGH OF RICHMOND UPON THAMES (SHARED USE) (LOADING PLACES AND TAXI RANKS) (No. 1) EXPERIMENTAL ORDER 2014

Made: 11 December 2014

Coming into operation: 5 January 2015

The Council of the London Borough of Richmond upon Thames, having consulted the Commissioner of Police of the Metropolis, in exercise of the powers conferred by Sections 9 and 10 of, and Part IV of Schedule 9 to, the Road Traffic Regulation Act 1984(a), as amended, and all other enabling powers hereby make the following Order —

1. This Order shall come into operation on 5 January 2015 and may be cited as the London Borough of Richmond upon Thames (Shared Use) (Loading Places and Taxi Ranks) (No. 1) Experimental Order 2014.
2. (1) In this order the following expressions have the meanings hereby respectively assigned to them:

“enactment” means any enactment whether public, general or local and includes any order, bye-law, rule, regulation, scheme or other instrument having effect by virtue of an enactment;

“the designated areas” means

 - (a) that area of The Quadrant, Richmond on the north-west side of that street as lies between a point 5 metres north-east of the north-eastern kerb-line of Quadrant Road and a point 24 metres to the north-east thereof, bounded on one side by the edge of the carriageway and having a width throughout of 2.7 metres; and
 - (b) that area of The Quadrant, Richmond on the south-east side of that street as lies between a point 7.5 metres south-west of the south-western kerb-line of Drummonds Place and a point 31.5 metres to the south-west thereof, bounded on one side by the edge of the carriageway, being fully on the footway and having a width throughout of 2.0 metres;

“the loading places order” means the London Borough of Richmond upon Thames (Loading Places) Order 1994(b) as amended;

“the cab ranks order” means the London Borough of Richmond upon Thames (Prohibition of Stopping on Cab Ranks) Order 1994(c) as amended;
- (2) Any reference in this Order to any enactment shall be construed as a reference to that enactment as amended, applied, consolidated, re-enacted by or as having effect by virtue of any subsequent enactment.

- (3) The Interpretation Act 1978(d) shall apply for the interpretation of this Order as it applies for the interpretation of an Act of Parliament.
3. Whilst this Order continues in force the provisions of the loading places order shall apply to the designated parking place as if:
- (a) in Article 3(1) of that Order the expression "loading place" is deemed to include the designated areas;
 - (b) the "permitted hours" (as used in that order) relevant to the designated areas means the period between 7am and 7pm throughout the week;
 - (c) the designated areas were deemed to be included in the provisions of Article 5 of that order; and
 - (d) Articles 6 and 7 of that order were interpreted as if the maximum period for which a vehicle may be left in the designated areas is twenty minutes.
4. Whilst this Order continues in force the provisions of the cab ranks order shall apply to the designated parking place as if:
- (a) in Article 2(1) of that order the expression "cab rank" was deemed to include designated areas and that the relevant road marking was a traffic sign as shown Diagram 1028.3 of the Traffic Signs Regulations and General Directions 2002(e) but without any associated legend;
 - (b) the "prescribed hours" (as used in that order) relevant to the designated areas means the period between 7pm and 7am throughout the week; and
 - (c) the designated areas were included in the provisions of Article 3 of that order.
5. In pursuance of section 10(2) of the Road Traffic Regulation Act 1984 the Assistant Director, Traffic and Transport of the London Borough of Richmond upon Thames, as the specified officer, or some person authorised in that behalf by that officer may, if it appears to that officer or to that person essential in the interests of the expeditious, convenient and safe movement of traffic, or of the provision of suitable and adequate parking facilities on the highway, or for preserving or improving the amenities of the area through which the road affected by this Order runs, modify or suspend this Order or any provision thereof.

Dated: 11 December 2014



Assistant Director, Traffic and Transport
(The officer appointed for this purpose)

(a) 1984 c. 27.

(b) 1994/03.

(c) 1994/60.

(d) 1978 c. 30.

(e) S.I. 2002 No 3113 – the relevant amending Instrument is 2011/3041.

EXPLANATORY NOTE

(This Note is not part of the Order but is intended to indicate its general purport.)

This Order introduces experimental shared use parking places in The Quadrant, Richmond. During the day (between 7am and 7pm) the parking places will be loading bays subject to a maximum stay of 20 minutes, and overnight (between 7pm and 7am) they will be taxi ranks.

TRAFFIC MANAGEMENT ORDER

THE LONDON BOROUGH OF RICHMOND UPON THAMES (WAITING AND LOADING RESTRICTION) (CIVIL ENFORCEMENT AREA) (No. 7) EXPERIMENTAL ORDER 2014

Made: 11 December 2014

Coming into operation: 5 January 2015

The Council of the London Borough of Richmond upon Thames, having consulted the Commissioner of Police of the Metropolis, in exercise of the powers conferred by Sections 9 and 10 of, and Part IV of Schedule 9 to, the Road Traffic Regulation Act 1984(a), as amended, hereinafter referred to as "the Act of 1984" and all other enabling powers hereby make the following Order —

1. This Order shall come into operation on 5 January 2015 and may be cited as the London Borough of Richmond upon Thames (Waiting and Loading Restriction) (Civil Enforcement Area) (No. 7) Experimental Order 2014.
2.
 - (1) In this Order the expression "enactment" means any enactment whether public, general or local and includes any order, bye-law, rule, regulation, scheme or other instrument having effect by virtue of an enactment.
 - (2) Any reference in this Order to any enactment shall be construed as a reference to that enactment as amended, applied, consolidated, re-enacted by or as having effect by virtue of any subsequent enactment.
 - (3) The Interpretation Act 1978(b) shall apply for the interpretation of this Order as it applies for the interpretation of an Act of Parliament.
3. Whilst this Order continues in force the provisions of the London Borough of Richmond upon Thames (Waiting and Loading Restriction) (Civil Enforcement Area) (Consolidation No. 8) Order 2008(c), as amended, shall have effect as though
 - (a) for the item numbered 551 in Schedule 1 to that Order there was substituted the item similarly numbered and set out in columns 1, 2 and 3 of Schedule 1 to this Order;
 - (b) there was added to Schedule 3 to that Order the item set out in columns 1, 2 and 3 of Schedule 2 to this Order.
4. In pursuance of section 10(2) of the Road Traffic Regulation Act 1984 the Assistant Director, Traffic and Transport of the London Borough of Richmond upon Thames, as the specified officer, or some person authorised in that behalf by that officer may, if it appears to that officer or to that person essential in the interests of the expeditious, convenient and safe movement of traffic, or of the provision of suitable and adequate parking facilities on the highway, or for preserving or improving the amenities of the area through which the road affected by this Order runs, modify or suspend this Order or any provision thereof.

(a) 1984 c. 27.

(b) 1978 c. 30.

(c) 2008/38

Dated 11 December 2014



Assistant Director, Traffic and Transport
(The officer appointed for this purpose)

SCHEDULE 1
(See Article 3(a))

ITEM TO BE SUBSTITUTED TO SCHEDULE 1 [to 2008/38]

1 Item No.	2 Street	3 Prescribed Hours – See Schedule 3 (to 2008/38)
551	Quadrant The, Richmond:	
	(a) the south-east side:	
	(i) from its junction with Sheen Road to a point 39 metres south-west of the south-western kerb-line of Drummonds Place;	A
	(ii) from a point 39 metres south-west of the south-western kerb-line of Drummonds Place north-eastwards for a distance of 31.5 metres;	Z56
	(iii) from a point 7.5 metres south-west of the south-western kerb-line of Drummonds Place to its junction with Kew Road;	A
	(b) the north-west side.	A

SCHEDULE 2
(See Article 3(b))

ITEM TO BE ADDED TO SCHEDULE 3 [to 2008/38]
PRESCRIBED AND RESTRICTED HOURS

1	2	3
Prescribed Hours – see Schedule 1 Z56	Street Between 7pm and 7am throughout the week.	Restricted hours - see Schedule 2 [to 2008/38] 82

EXPLANATORY NOTE

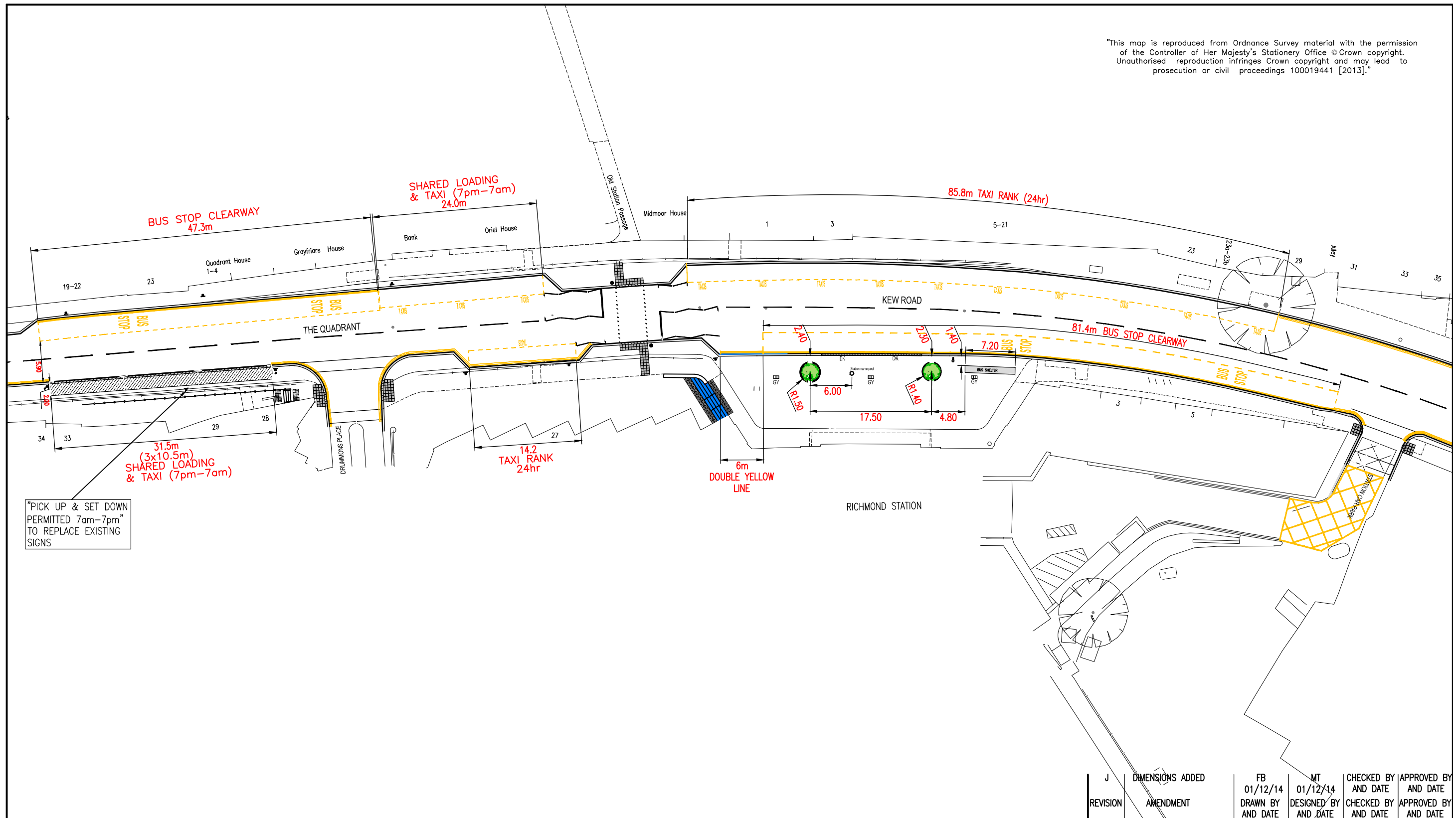
This Order amends a length of waiting restriction in The Quadrant, Richmond to prohibit waiting between 7pm and 7am throughout the week except for taxis.

STATEMENT OF REASONS

RICHMOND STATION TAXI RANKS AND SHARED USE TAXI RANKS AND LOADING BAYS

The Council, in consultation with representatives of taxi operators in the borough, Transport for London, South West Trains and other interested parties is making changes to the road layout in the vicinity of Richmond Station. This will involve significant changes to bus stops, taxi ranks and loading bays. The orders introduce new cab ranks and shared use cab ranks and loading bays. These measures provide an improved and safer environment for all road users and passengers using the local public transport links.

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J	DIMENSIONS ADDED	FB	MT	CHECKED BY	APPROVED BY
REVISION	AMENDMENT	01/12/14	01/12/14	AND DATE	AND DATE
		DRAWN BY	DESIGNED BY	CHECKED BY	APPROVED BY
		AND DATE	AND DATE	AND DATE	AND DATE

		<input checked="" type="checkbox"/> DRAFT <input type="checkbox"/> ISSUE	SCALE (AT A3 SIZE) NTS
CLIENT	LONDON BOROUGH OF RICHMOND UPON THAMES		
PROJECT	RICHMOND TOWN CENTRE		
DRAWING TITLE	PROPOSED SIGNAGE/MARKINGS ALTERATIONS		
ISSUING OFFICE	TWICKENHAM	DRAWING NUMBER	3010.DD.1200.002
TELEPHONE	08456 122 660	REVISION	J

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